

**CAREC Transport Corridor 1 (Bishkek-Torugart Road) Project
(km 400 – km 439), At-Bashy village access road**

Due Diligence Report



**March 2012
Investment Projects Investment Group of MOTC**

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ABBREVIATIONS

ADB	Asian Development Bank
BNT	Bishkek-Torugart Road
CAREC	Central Asia Regional Economic Cooperation
CRBC	China Road and Bridge Corporation
DDR	Due Diligence Report
IPIG	Investment Project Implementation Group (former PIU)
JOC	Japan Overseas Consultants
LAR	Land Acquisition and Resettlement
LARP	Land Acquisition and Resettlement Plan
MOTC	Ministry of Transport and Communications
PIU	Project Implementation Group (present IPIG)
PRC	People's Republic of China
DEP	Road Maintenance Unit
RoW	Right of Way

1. Introduction and Background Information on the Project

1. The government of the Kyrgyz Republic attracts international loans and grants for implementation of the project for rehabilitation of Bishkek-Torugart road (BNT). Bishkek–Torugart road is part of the Central Asia Regional Economic Cooperation (CAREC) Corridor 1 linking the Kyrgyz Republic with the People’s Republic of China (PRC) and other Central Asian countries. In addition to promoting international trade and economic cooperation, Project also provides for direct and indirect benefits to 2.26 million people including 51% women living along the project road, mainly engaged in trade and commercial activities both within the region and on the inter-district and inter-oblast levels. The project road passes through three oblasts: the Chui, Issyk-Kul and Naryn. In two of the three oblasts, where the road project passes, the poverty level is above the average for the whole country, which makes 46%. The implementation of this Project will open up this corridor for regional trade and economic cooperation. It will reduce travel and transit times from the current 3–4 days to 2 days.

2. The road condition is poor; border-crossing facilities and procedures are outdated and inefficient, which obstruct international traffic and trade, and regional economic cooperation. Rehabilitation of the existing carriageway is the basis of the project road.

3. As part of the BNT Rehabilitation Project, ADB provided a grant in the amount of USD 20 million for the rehabilitation of the road section between km 400 and km 439 (note: ADB financed Project 1). At present the project at the stage of completion and there is an unused USD 780 thousand as part of this project. In view of the above said, it is expected to allocate this amount for the rehabilitation of existing At-Bashy access roads, the district center. This road is a road inside the village At-Bashy and begins at km 385 of the main BNT road with the exit at km 390. The entire length of the road from the entrance to the village of At-Bashy and exit is 10.4 km. Request has been submitted to ADB to consider the possibility of reallocation of funds from the Category of Goods (customs component) to the Category of Works for the purposes of rehabilitation of the road. Although, a definitive answer was not received from the ADB, the Bank advised to prepare DDR, in order to guarantee that there will be no impact on the present section of the road.

4. The current state of the At-Bashy access roads.

- Asphalt covering from km 0 to km 4 of the road requires patching in some areas;

- On the way down to the village At-Bashy, road has the improved gravel surface;

- Directly in the village At-Bashy road has asphalt covering with potholes and cracks on entire length until the exit from the village;

- The road, on entire length, is flooded with melt water due to the lack of normal water drainage;
- Shoulder of the road is washed away by melt water and eroded in some places;
- In places where the culverts are located the soil is eroded, the culverts are blocked with soil;
- In some places, the length of the culverts is shorter than the width of the proposed road;

The harsh winter of this season had a negative impact on the pavement condition. Sharply continental climate of the region has a negative impact on the roads condition. In March 2012, daytime temperatures were about +1 to +3 degrees of Celsius, and night minimum falling down to -8 to -10 degrees of Celsius.

5. It is assumed that the road will be of Category IV with a width of roadway 10 m. The civil works will include the following activities:

- a. repair of hydraulic and drainage system in order to protect the roadway;
- b. patching works and crack pouring;
- c. building of a single-layer asphalt covering, where necessary.

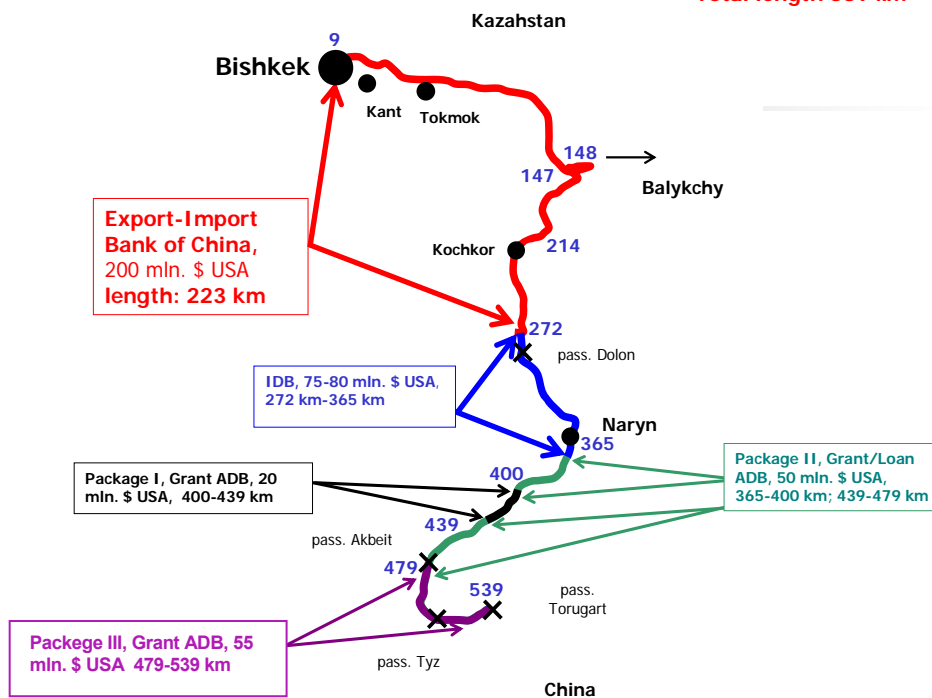
6. For the preparation of the DDR, the IPIG safeguards team visited the site and examined in details each section of the road with buildings and / or structures, and trees within the projected road. In addition, the team met with representatives of state raion administration, local self-government and DEPs to discuss the impact on the separate sections. All the above individuals have supported the planned rehabilitation of the road and looked forward to soonest implementation.

7. During the preparation of DDR, it was revealed that all the buildings and trees along the road are at some distance from the road and there is no need of resettlement, green are clearing and land acquisition. The table on the LAR is attached to this report (Table 1).

Below is a map of Bishkek-Naryn-Torugart

Bishkek-Naryn-Torugart Road

Total length 539 km



Source: Ministry of Transport and Communications of the Kyrgyz Republic.

Access road to At-Bashy village





Km 0+000

Turn from main road

Joint with main road

Km 10+420

Bridge over river

Km 9+525

Entry to At-Bashy village

Km 4+415

Km 9+030

Last turn to the exit from village

2. Baseline Legal Data and the Grievance Redress Mechanism

1. Strategic Framework of the Project is based on ADB's Policy on Involuntary Resettlement (1995), ADB's Safeguards Policy Statement (2009) and Operations Manual (OM) Section F1/BP (2010), the Constitution of the Kyrgyz Republic, the Land Code (LC) and the Civil Code (CC) of the Kyrgyz Republic, where there are differences between local law and ADB practice. As a rule, strategic differences will be resolved in favor of the latter¹

2. The Constitution of the Kyrgyz Republic was adopted in June 2010. It is the main legal instrument to ensure the rights of citizens. Article 12 stipulates that "The diversity of forms of ownership is recognized in the Kyrgyz Republic and equal legal protection of private, state, municipal and other forms of property is guaranteed. Seizure of property for public purposes, as defined in the law, may be executed by the court's decision in order to ensure fair and prior compensation for the value of this property and other damages caused as a result of alienation. The land may also be in private, municipal and other forms of property, except for pasture, which cannot be privately owned."

3. Compensation for land acquisition and other impacts of public projects are regulated by other legislative acts, including the Land Code (LC) and the Civil Code (CC). Based on these laws, land acquisition and resettlement are based on the following rules:

(i) Land users and landowners have the right to demand compensation for losses resulting from removal of their rights to land use for public and social needs (Articles 49 and 66 LC).

(ii) Upon expiration of the right to property, the property shall be assessed on the basis of its market value (Article 68 LC).

(iii) The state authorities shall inform the public on the seizure and land acquisition for construction that affect the interests of the population.

4. Land Code (1999) is the main legal instrument for the land acquisition. It has been updated several times; the latest amendments were introduced in 2009. Paragraphs 62 and 66 of the LC define the mechanisms for finalization and voluntary transfer of land on the following grounds:

- The use of land for unintended purpose;
- The land is needed for the state or public needs;
- The land designated for agricultural purposes is not used within three years;
- The land designate for non-agricultural production is not used in the period specified in the original provision;

¹ If an international agreement ratified by the Parliament of the Kyrgyz Republic defines the conditions, which differ from the ones established according to the Kyrgyz legislation, the regulation of an international agreement prevails (Article 6, Civil Code 1999)

- Failure to pay land taxes;
- Failure to pay social taxes;
- Abatement of the license for the development of deposit on the basis of the legislation of the Kyrgyz Republic “On mining.”

5. Paragraph 49 of the Land Code states the rights of landowners: the owners should receive full compensation for the loss in the cases stipulated by the Kyrgyz legislation. Item 68 of the LC appeals to ensure that private owners have received financial compensation from the loss of land, or provided with the other land, equal to the total market value. If agreement on compensation cannot be achieved, the responsible agency may declare the land acquisition and compensate the landowner or land user in accordance with the court decision within two months (Item 68, Article 1).

6. Basic principles of ADB’s Policy on Involuntary Resettlement:

- Involuntary resettlement should be avoided or minimize by exploring all viable project options;
- Identification of affected persons and compensation for lost property and income;
- Assistance in resettlement and restoring the living standards to the level without the project;
- The affected people should be fully informed and closely consulted on resettlement and compensation options;
- The absence of formal legal title should not be a bar to compensation or assistance in rehabilitation of livelihood;
- Pay special attention to particularly vulnerable groups.

7. In addition, Grievance Redress Group (GRG) will be established according to ADB's Safeguards Policy Statement. GRG will allow local residents to complain and make appeals that may arise during the implementation of the project. GRG will consist of the following three levels:

Local Level: The complaint is originally filed and considered at the village level. The applicant brings his complaint to the local authorized person, who is appointed by the Minister of Transport and Communications and is responsible for the timely consideration of issues and complaints. The authorized person shall agree measures to resolve complaint together with DEP, IPIG, representatives of local authorities and local NGOs. After coordination with the IPIG, the authorized person shall take the final decision on the complaint and determine the admissibility of the complaint. Local authorized person for this project - Assistant of Resident Engineer, Ibraev Keldibek, mobile: +996 555 189504

Regional Level: If the complaint is not addressed within 15 days, the applicant, with the assistance of NGO representatives, submits a complaint to the regional division of MOTC (representatives of IPIG and RCMD BNT). After coordination

with the IPIG, the representative of IPIG takes the final decision on the complaint and determines the admissibility of the complaint.

Central Level: If the complaint is not addressed within 15 days, the applicant shall send it to the central office of IPIG MOTC KR. Representatives of the IPIG shall make the decision on the eligibility of the complaint.

Court: If the complaint is not solved within 15 days at the Central Level, the applicant may submit the complaint to court.

3. Methodology and Data Collection

8. Present DDR has been prepared for the existing At-Bashy access roads based on the ADB recommendation. For the preparation of the DDR, the safeguards team comprised of IPIG experts A. Keshikbaev, S. Keldibaeva and JOC inspector M. Beksultanov visited the site and thoroughly examined every section of the road with buildings and / or construction as well as trees located near the project road. In addition, the safeguards team photographed all buildings, structures and trees in proximity to the road from different points.

9. Methodology of data collection for DDR includes:

- Collection and review of relevant reports such as detailed designs, feasibility study and other information provided by MOTC;
- Review of statistical reports prepared by At-Bashy department of the National Statistical Committee;
- Review of documents prepared by ADB, United Nations, World Bank and other agencies;
- Consultation with relevant MOTC staff;
- Consultations with builders;
- Consultations with permanent residents in the project area, and
- Field research along the entire road over March 12 to March 15, 2012.

10. The reconstructed road is subject to ADB's Safeguards Policy Statement for which this report is being prepared.

4. Project Impact Assessment

A) km 0+000 – 4+810

B) km 4+810 – 9+030

C) km 9+030 – 10+420

A) km 0+000 – 4+810

There are very few permanent structures on the first 4 km of the road. Lengthwise of the road, there are mainly trees, shrubs, telegraph poles, power line poles, which are not likely to be affected by the project.

B) km 4+810 – 9+030

Capital structures and homes, roadside service and trade facilities start from km 4+810. All these facilities are located beyond the carriageway. The distance from the road edge to these objects ranges from 2 to 8 meters.

Section from km 6+640 to km 7+150 is the subsection of the road with the most intense traffic passing through the center of the village At-Bashy, where the building of At-Bashy raion administration and the bazaar area are located. There is a large concentration of vehicles, people and it is the busiest section of the road. Nevertheless, as seen from the photos below, all trading facilities are behind the road curbs and thus they may not be subject to impact.

Section from km 7 to km 9 road runs on the less intensive subsection, where all the permanent structures are located at some distance from the edge of the road, thereby, eliminating any impact.

C) km 9+030 – 10+420

There are no facilities and green spaces, except for the cemetery at km 9+79, on the road section from km 9 to the end of the road at 10+420. Distance from the edge of the road to the nearest graves makes 8 meters, which eliminates any impact. Km 10+420 is a section of road, where the road from village At-Bashy adjoins to the main road. From the beginning of the BNT Project 1, local residents have repeatedly been asking to rehabilitate At-Bashy access road under consideration, because it is the main central road in the village At-Bashy.

5. Summary Table of Impact Assessment

The table below summarizes the above findings with the results of the possible impacts of resettlement.

Photo No.	Picket	Objects	Possible impact
1	Subsection km 0+000 – 4+810		
1	0+000		None
2	0+150 (on the left)	Telegraph pole	None
3	0+300 (on the right)	Telegraph pole	None
4,5	0+680 (on the right)	Roadside plantings	None
6,7	0+700 (on the left)	Roadside plantings	None
8,9,10	0+715 (on the right)	Roadside plantings Residential house with outhouse extensions at a distance of 200 m from the edge of the road	None
11,12	1+060 (on the left)	Roadside plantings	None
13,14	1+130 (on the right)	Roadside plantings	None
15	1+220 (from both sides)	Shrubs along the road	None
16,17	1+270 (on the right)	Roadside plantings	None
18,19	1+305 (on the left)	Roadside plantings	None
23,24	4+300 (on the left)	Wire fence of a residential house at a distance of 9.5 m from the edge of the road	None
25,26	4+340 (on the right)	Roadside plantings	None
2.	Subsection km 4+810 – 9+030		
29,30	4+810 (on the right)	Gas station at a distance of 11 m from the edge of the road	None
36,37	5+090 (on the right)	Fence (wooden) of the residential house	None
45,46	5+485 (from both sides)	On the left is an old construction trailer, on the right are the fences of the residential houses	None
47,48	5+590 (from both sides)	On the left is the monument for Azhibek Baatyr, on the right are tree plantings	None
51	5+910 (on the left)	Shop-kiosk	None
52	6+235 (on the left)	On the left is the water conduit well	None
53-65	6+640 – 7+160	Building of raion state administration, trading facilities, bazaar, tree plantings, high	None

		concentration of vehicles and people	
66	7+215 (on the left)	Tree plantings	None
68	7+380 (from both sides)	On the left is the residential house, on the right is gas station	None
70,71	7+500 (on the right)	Kiosk "PAOLO"	None
72	7+700 (on the left)	Bus stop sheds	None
73	7+940 (on the right)	Corner of the residential house	None
80,81	8+290 (from both sides)	On the left is the brick fence, on the right is a wire fence	None
83,84	8+510 (from both sides)	On the left is the residential house, on the right is power line pole	None
86,87	8+660 (from both sides)	On the left is communication line pole, on the right is residential house	None
89,91	8+750 (on the right)	Fence of a residential house	None
93	8+985 (on the right)	Fence of a residential house	None
3.	Subsection km 9+030 – 10+420		
98,100	9+790 (on the left)	Cemetery	None

6. Conclusion and recommendations

17. As described above, all buildings and structures along all sections of the road are outside the project impact zone and will not be affected in the process of road construction works. Resettlement and land acquisition are not expected along the entire length of the road, as well as the forced cutting of plantings. Inside the village, none of the fences, residential houses or green areas will be impacted along the main street of the raion center. However, there are some areas with closely located tree plantings, electric poles and lines, which should be addressed as part of the designing without any issues of resettlement and land acquisition; moreover, the above mentioned objects are at a safe distance from the road. Issues related to reconstruction of man-made structures, in particular the conduits, which have a shorter length than the width of the road should be addressed closely. Moreover, the issues with the improvement of drainage systems to prevent adverse impacts on the roadbed must be carefully worked through.

Thus, the implementation of the proposed project will have no adverse impact on the owners of closely located residential properties and infrastructure facilities, and green are clearing is not expected.

7. List of persons discussions and consultations were held with

#	Names	Address	Position
1.	Asanbekov S.A.		Chief Engineer, RCMD BNT
2.	Abyov K.	v. At-Bashy	Head of DEP-957
3.	Kurmanbekov N.	v. At-Bashy	Acting Head of DEP-20
4.	Tentimishhev Zh.R.	v. At-Bashy	Deputy Head of At-Bashy raion state administration
5.	Zhumabekov T.	v. At-Bashy	Head of raion architecture department
6.	Akunov Zh.	v. At-Bashy	Chief Engineer, DEP-957
7.	Zhumakov O.	v. At-Bashy	Head of At-Bashy Aiyl Okmotu
8.	Osmonov Beishenbek	25 Arpa Street, v. At-Bashy	Pensioner
9.	Zholdoshbek uulu Almaz	16 Arpa Street, v. At-Bashy	Pensioner
10.	Zhumabaev Zhyrgalbek	8 Bazar Street, v. At-Bashy	Pensioner
11.	Asanbaev Asan	132 Shakhnobiev Street, v. At-Bashy	Pensioner
12.	Satiev Seiden	17 Chech-Tobo Street, v. At-Bashy	Pensioner, former Head of Aiyl Okmotu
13.	Sydykov Zhenishbek	55 Ak-Sai Street, v. At-Bashy	Pensioneer
14.	Akunov Melis	140 Shakhnobiev Street, v. At-Bashy	Accountant, raion power network
15.	Karbekov Akylbek	276 Chatyr-Kul Street, V. At-Bashy	Private entrepreneur
16.	Karabaev Kanatbek	31 Aiylchiev Street, v. At-Bashy	Pensioneer, former secretary of Aiyl Okmotu
17.	Tabaldiev Medet	89 Ak-Sai Street, v. At-Bashy	Head of “Aiyl Bank” Branch
18.	Kurmanaliev Zhyrgalbek	92 Ak-Sai Street, v. At-Bashy	Chief accountant of “Settlements & Savings Bank”

8. Annex (photos)



Photo 1 The turn-off from the main road to At-Bashy village. Zero reference point. The blue cone – indication of the center line.



Photo 2 The telegraph pole on the left is marked 5 m from the center line.



Photo 3 The telegraph pole on the right is marked 5 m from the center line.



Photos 4 and 5 Roadside plantings. The specified point on the right has a tree growing at a distance of almost 1 m from the road edge.



Photo 6 and 7 Roadside plantings. The specified point on the left has a tree growing at a distance of almost 1 m from the road edge.



Photo 8 and 9 Roadside plantings. The specified point on the right has a tree growing at a distance of almost 1 m from the road edge.



Photo 10. A residential house with an extension at a distance of 200 m from the edge of the road to the right.



Photos 11 and 12 Roadside plantings. The specified point on the left has a tree growing at a distance of almost 1 m from the road edge.



Photos 13 and 14 Roadside plantings. The specified point on the right has an isolated bush growing at a distance of almost 1 m from the road edge. High-voltage power line pole at a distance of 10 m from the road edge on the left.



Photo 15 Roadside plantings. The specified points on the left and on the right have isolated bushes growing at a distance of almost 1 m from the road edge



Photos 16 and 17 Roadside plantings. The specified point on the right has 4 isolated trees at a distance of 0.5 m from the road edge



Photos 18 and 19 Roadside plantings. The specified point on the left has an isolated tree at a distance of almost 1 m from the road edge. No objects that may become subject to the project impact are located along the road on the section from km1+305 to km 3+120



Photo 20 The projection featuring the road and the vicinity on km 3+120. The shot depicts no objects along the road that may become subject to the project impact.



Photo 21 The projection featuring the road and the vicinity on km 3+700. The shot depicts no objects along the road that may become subject to the project impact.



Photo 22 The projection featuring the road and the vicinity on km 4+150. The shot depicts no objects along the road that may become subject to the project impact.



Photos 23 and 24. The wire fence on the left is located at a distance of 9.5 m from the road edge and a residential house with a cob fence is located 10-12 from the wire fence



Photos 25 and 26. A wire fence of an allotment is located at a distance of 4.5 m from the right edge of the road



Photos 27 and 28 The entrance to At-Bashy village



Photos 29 and 30 The concrete blocks are located 1 m from the road edge and a gas station is 8-10 m behind the blocks



Photo 31 The carriageway width is narrowed by concrete curbs on both sides



Photos 32 and 33. 1.5-2 m wide roadside pedestrian walks are seen on the various specified points. A shed with a cob fence is located at a distance of 5 m from the road edge at km 4+900 on the right



Photos 34 and 35 10 m wide curbs are evidenced along the existing road on the points specified above



Photos 36 and 37 The left shot depicts the center line (the blue cone) and the road width of 10 m. The right shot depicts a wooden fence of a residential house at a distance of 2 m from the road edge



Photo 38 The center line and the road width of 10 m are depicted on this shot. The sidewalks are not always paved and are mostly deteriorated, sometimes they have earth pavement and at some points may not be seen due to the snow banks



Photos 39 and 40 The condition of the sidewalks can be seen on the right and on the left sides of the road, the sidewalks are covered with snow and damaged in some areas. A cob fence on the left is 2 m away from the road edge.



Photos 41 A center line and 10 m width of the road are depicted on the above shot. 2m wide sidewalks are seen to be outside the road. Photo 42 The fork of the road, Sartbai Abykeev street, the fence on the left is 4 m away from the road edge.



Photos 43 and 44. 15, Toktorbay Omurzakov street . The fences and the stone edges are to the left and to the right of the road. No permanent structures are in close proximity to the road and will not be subject to the project impact.



Photos 45 and 46 An old construction trailer is located 4 m away from the left side of the road. The picture on the right depicts the location of the residential house fences 5 m away from the road edge



Photos 47 and 48. 10 m wide paved carriageway section



Photos 49 and 50. 5-8 m distance is evidenced to left from the road edge to the fences and residential houses. The same situation is proved on the right side from the road edge. The curbs, roadside planting, ditches and wide sidewalks are depicted on both sides.



Photo 51 A permanently structured business kiosk 3 m away from the road edge on the left is located on km 5+910. Photo 52 A center line (the blue cone) and the 10 m road width *дороги*. A water-conduit well located 1 m away from the left edge of the road



Photo 53. 10 m road width is evidenced on the opposite side of the Rayon State Administration building



Photos 54 and 55 The concrete curbs, wide sidewalks, roadside planting that will not become subject to the project impact are observed in the center of the village.



Photos 56, 57, 58, 59, 60, 61, 62 and 63 The rayon marketplace is located in the center of the village, the measurements were not possible due to the high traffic intensity. However, the distance between the curbs makes 10 m and is seen on the pictures above. The vehicle parking spaces, the sidewalks, plantings and etc. are observed to be outside the road carriageway.



км 7+150



км 7+160

Photos 64 and 65 The road bend to Maatkabyl Salymbekov street. The sidewalks to the right and to the left from the road and the 10 m road width are indicated on the pictures above



км 7+215



км 7+215

Photos 66 and 67 The shot made along Maatkabyl Salymbekov street. The trees on the left and on the right are 2 m away from the road and the houses are seen to be 7 m away from the road edge.



км 7+380



км 7+500

Photo 68 The turn to the left from Maatkabyl Salymbekov street. Residential house is 7 m away from the road edge on the left. A gas station is 8 m away on the right. Photo 69 General view of the road from the turn



Photos 70 and 71 A fence of residential house is 8 m away on the left. «PAOLO» Business kiosk established on the bus-station is at a distance of 4 m from the edge of the road



Photo 72 The bus-station sheds are 3 m away from the road edge and further 30 m along the road on the right. Photo 73 The center line and 10 m width of the road. The angle of the residential house fence is 3 m away from the road edge



Photo 74 General view of the road at an entrance to Ak-Jar village. Photo 75 The shot of the drainage channel crossing. The carriageway width is 8.5 m



Photos 76 and 77 The crossover of the road with the drainage channel is both on the left and the right shots, the carriageway is slightly narrowed (down to 8.5 m)



Photos 78 and 79. 10 m road width and the center line of the road at a turn to Bosbai street. The sidewalk space is seen on the left and the right sides from the road edge



Photos 80 and 81 The road edges on both pictures (5 m from the center line). The brick and the wire fence at a distance of 3 to 5 m from the road edge. The shot is made at 11, Bosbai street.



Photo 82 Bosbai street and Akmat Jarkinbaev street crossroad. 10 m road width and the center line (the blue cone)



Photo 83 Residential house 4 m and a domestic power line 3 m away from the left road edge.
Photo 84 Wooden fence at a distance of 1 m from the road edge.



Photo 85 A turn from the Bosbai street to the left (house No 25)



Photos 86 and 87 Communications and power line pole is 3 m and a residential house fence is 4 m from the road edge. Residential house No.25 is located 5 m to right from the road edge.



Photo 88 The turn from Bosbai street to Janusak street (on the opposite side of house No. 38). The centerline and 10 m width are shown on the picture. Photo 89 General view of Janusak street



Photos 90 and 91. 2-3 m up to residential house fences from both sides of the road edge with the sidewalk



Photos 92 and 93. Turn to the right from the street N. Zhanuzak to the street leading to the exit from the village to the main road. The photo shows the road width of 10 m and its axis (blue cone). Sidewalks are to the left and right of the road edge and further are the fences of residential houses and administrative buildings (District Department of Forestry)



Photos 94 and 95. General view of the road with the indication of 10 m width and its axis. To the left and right, there are no objects that will be impacted by the project



Photo 96. Exit from the village. Concrete power line poles at a distance of 4 m and extensions fence 5 m on the right from the edge of the road. Photos taken before the bridge and cemetery. Photo 97. Measurements on the bridge. The width of the carriageway is 8 m



Photo 98. Exit from the village. Photo taken opposite to the cemetery.

Photo 99. Nearest cemetery graves are located to the left of the road edge at a distance of 8 m.



Photo 100. General view of the road. The main road is visible on the horizon.

Photo 101. Adjoining of At-Bashy village access road with the main Bishkek-Naryn-Torugart Road